



Report of the Chief Planning Officer

CITY PLANS PANEL

Date: 23 JULY 2015

Subject: PLANNING APPLICATION REF. 15/02470/FU FOR DEVELOPMENT OF NEW SECONDARY FREE SCHOOL WITH ASSOCIATED SPORTS AND RECREATION FACILITIES, PARKING AND LANDSCAPING AT BLACK BULL STREET, LEEDS LS10

APPLICANT	DATE VALID	TARGET DATE
BAM Construction Ltd	01.05.2015	31.07.2015

Electoral Wards Affected:

City and Hunslet

Yes Ward Members consulted
(referred to in report)

Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

RECOMMENDATION:

DEFER and DELEGATE to the Chief Planning Officer for approval in principle, subject to the resolution of the following detailed matters:

- surface water drainage discussions with Yorkshire Water
- agreement of the commuted sum calculation towards the east-west pedestrian link at the southern end of the site
- the travel plan including measures regarding pupil pick up and drop off
- confirmation that all off-site highways works are acceptable
- confirmation that the construction management plan is acceptable
- confirmation that the concerns of West Yorkshire Combined Authority regarding relocation of bus stops and school bus provision can be addressed

and the specified conditions (and any others which he might consider appropriate), and following the completion of a Section 106 Agreement to cover the following matters:

- Travel plan monitoring fee £2,500
- Local bus stop improvement £20,000
- Community use of facilities

- **Public access to east-west link across the northern end of the site**
- **Committed sum towards the future delivery of an east-west pedestrian route to the south of the site (sum to be agreed)**
- **Cooperation with local jobs and skills initiatives**

In the circumstances where the Section 106 Agreement has not been completed within 3 months of the resolution to grant planning permission the final determination of the application shall be delegated to the Chief Planning Officer.

1.0 INTRODUCTION:

- 1.1 This application is brought to Plans Panel because it is a large-scale major planning application, which if approved would deliver a new educational facility for inner Leeds, and promote the regeneration of a large brownfield site in the South Bank of the City Centre and the Aire Valley regeneration area.
- 1.2 The Ruth Gorse Academy and their professional team made a pre-application presentation at Plans Panel 16th April 2015, and Members were generally supportive of the principle of the scheme, subject to the resolution of the detailed matters set out at paragraph 5.2 of this report.

2.0 PROPOSAL:

- 2.1 The Ruth Gorse Academy is a Free School sponsored by The GORSE Academies Trust, a not-for-profit charity creating exceptional schools in areas of deprivation within the Leeds City Region. The development of the Academy at this site would add to the growing educational cluster in the South Bank with the nearby Leeds City College Printworks Campus and the Leeds College of Building. The Academy is keen to work with both colleges to deliver vocational training to prepare pupils for the workplace and further /higher education. The Academy state that there will be exponential growth in the secondary sector in Leeds over the next seven years, and without this free school it is forecasted that there would be a shortfall of approximately 1,250 secondary places in September 2018/19.
- 2.2 The Ruth Gorse Academy would focus on ensuring that young people currently living in the inner south area of Leeds can have access to a new secondary academy. It would be a large academy of 1580 students and would mirror the aspirations, expectations and standards at The Farnley Academy and The Morley Academy. For the next two years the Ruth Gorse Academy would be based on The Morley Academy site (part of the GORSE Academies Trust), acting as a completely separate school with its own core staff, buildings and entrance to the school. These premises opened in September 2014 with an intake of 95 students in year 7, increasing to a total of 285 students in September 2015 for years 7 and 8. Subject to the planning process, from September 2016, the Academy would operate from a new complex on Black Bull Street, with student numbers building from 527 in September 2016 to full capacity in September 2018. When at full capacity, the Ruth Gorse Academy would include approximately 1580 students and 150 full time and part time staff.
- 2.3 The Academy would comprise a new U-shaped building with a gross area of 12,234 square metres gross internal area, formed of three main elements. These joined buildings would provide teaching accommodation and support facilities arranged around a south facing courtyard:
 - The Hub block, a three storey glazed and dark grey metal clad building would front the car park and house the Library Resource Centre (LRC), dining,

administration, services/deliveries, ancillary space, activity studios, the sports hall, and the main hall. The building features recessed window reveals with glazed slots, a fully glazed ground floor treatment to Black Bull Street, and a metal fascia detail which would act a transition to the upper level cladding. It also features a metal external escape ramp from the first floor sports hall and main hall in matching materials;

- The STEM (Science/Technology/Engineering//Maths) block, a three storey red brick and glazed block fronting Black Bull Street, which would house Maths, Science, Innovation, Design and Art. The building would feature deep window reveals and rooftop solar panels, which would be expressed to create a distinctive traditional warehouse roof-form similar to that at the Leeds College of Building;
- The Humanities block, a four storey glazed and light grey render building set slightly back from the Chadwick Street frontage, which would house English, Humanities, Drama, Modern Languages, Music, and ICT. The building would feature grey metal cladding to the southern gable and at floor spandrel levels. It would feature rooftop solar panels behind the parapet;
- The Spine block would link all three elements and front onto the internal south facing courtyard. The main entrance to the school would be marked by a glazed slot between the STEM and Hub blocks. It would be clad in dark grey metal cladding with glazed curtain walling to the south elevation.

2.4 The scheme would provide two external dining areas, a growing area with fruit trees, external games and performance area, a 100m running track, a formal hard surfaced playing pitch, and three Multi-Use Games Areas (MUGAs)

2.5 The scheme includes 41 car parking spaces for staff and visitors, 20 staff and visitor cycle spaces, 8 motorcycle spaces, and 2 mini-bus parking spaces. 40 cycle parking spaces are proposed for pupils, with space for future expansion if initiatives to encourage cycling are taken up (50 more spaces). One electric vehicle charging point is also proposed.

2.6 The frontage to Black Bull Street would feature 12 new trees. The boundary treatment to the car park area to the north would be a low hedge with vehicular access controlled by barrier. Low hedges would also demarcate the 3m wide pedestrian and cycle route to the NGT stop and Leeds Dock beyond. The Chadwick Street boundary would consist of retained brick piers and railings, and the southern boundary of the site would be formed by the 3m MUGA fencing and roof netting, and new 2.1m high railings to the remainder. Brick piers and railings would provide a secure infill between the end of the STEM block and the MUGA.

2.7 The normal school day would be 08:25 – 14:45, with pre-school breakfast clubs and extracurricular activities commencing from 07:20. A number of after school enrichment activities means most students will remain on site until at least 16.30, with some activities concluding at 22:00 or in some cases 23:00 for certain events such as school performances.

2.8 The pupil intake at The Ruth Gorse Academy will mainly come from areas of the southern part of Leeds such as:

- Beeston Hill
- Beeston
- Belle Isle

- Cross Green
- Holbeck
- Hunslet
- Middleton
- Stourton

2.9 A number of documents were submitted in support of the application:

- Scaled Plans
- Planning Statement including Section 106 Heads of Terms
- Statement of Community Involvement
- Design & Access Statement
- Sustainability Statement
- Desk Top Archaeological Report
- Noise Assessment
- Air quality assessment
- Transport Assessment
- Travel Plan
- Drainage Plans
- Flood Risk Assessment
- Flood Risk Sequential Assessment
- Land Contamination and Remediation Reports
- Coal Recovery Report
- Construction Management Plan

3.0 SITE AND SURROUNDINGS:

- 3.1 The 1.8ha brownfield site is located to the east of Black Bull Street at the former Yorkshire Chemicals site. The site lies unallocated within the City Centre and within flood risk zone 3. The surrounding area is a mixture of uses including residential, offices, food and drink and retail, at Brewery Wharf, Leeds Dock, Leeds City Office Park and Crown Point Retail Park. The nearest residential to the site is along the eastern side of Chadwick Street at Leeds Dock. A car showroom and workshop lies to the south of the site, along with unlisted heritage asset The Malthouse on Chadwick Street, now in office use. The Grade II* listed Chadwick Lodge lies at the junction of Crown Point Road and Black Bull Street to the north-west of the application site. Immediately to the north lies a vacant 2 storey red-brick office building at 48-50 Black Bull Street. To the west lies the vacant former Tetley Brewery site at Waides Yard. The Leeds College of Building campus on the western side of Black Bull Street is also now complete and occupied. The conversion of the Grade II listed Alf Cooke Printworks to form a campus for Leeds City College on Hunslet Road has opened and phase two is currently underway. The site lies just outside the South Bank Planning Statement area, but could have an important strategic role in linking the City Centre core, the railway station and the future City Centre Park to visitor attractions, convenience shops, food and drink, homes and workspace at Leeds Dock.
- 3.2 The area is planned to be served in the future by the proposed NGT trolleybus system, with the nearest stop located on Chadwick Street directly adjacent to the site.
- 3.3 The site lies within the designated Aire Valley Leeds Urban Eco-Settlement. The scheme has potential to provide much needed educational facilities for the existing residential community in inner south Leeds and the City Centre, and for the planned new housing provision (over 6500 homes). The scheme also has strong potential to

contribute to the place-making of the South Bank, by bringing a long vacant site into active use.

4.0 RELEVANT PLANNING HISTORY:

- 4.1 Detailed discussions with the Council regarding this site have taken place since mid-2005 following the closure and subsequent demolition of Yorkshire Chemicals. This led to the submission of an outline planning application in 2006 (ref. 06/04601/OT), which was approved at Plans Panel (City Centre) in 2008, and subsequently granted permission in 2009 for a multi-level mixed use development comprising predominantly residential (678 flats and 43 townhouses), with office, hotel, leisure, retail, car showroom, community uses, public space and car parking. This permission expired in July 2012.
- 4.2 BAM Construction Limited have also recently submitted planning and listed building applications for the change of use of part of the nearby Braime Pressings factory on Hunslet Road, to a new University Technical College (UTC) including additional internal floors, new entrance, stair block and alterations to roof and windows (planning references 15/03836/FU & 15/03837/LI). Braime Pressings will still operate their business in the remainder of the Grade II listed building. Subject to planning and listed building consent, Leeds UTC will specialise in Advanced Manufacturing and Engineering, Science, Technology, and Mathematics for 14-19 year olds via project work, work experience, vocational and academic learning. The scheme is supported by local employers including Siemens, Kodak and Agfa, University of Leeds, Leeds City College, Leeds City Council, EEF The Manufacturers Organisation, Leeds Chamber of Commerce, Grant Thornton and over 25 other local businesses. If approved the UTC would add to the existing further education and vocational training facilities at Leeds City College Printworks Campus and Leeds College of Building Hunslet Road Campus.

5.0 HISTORY OF NEGOTIATIONS

- 5.1 Officers had two formal pre-application meetings with the Ruth Gorse Academy's professional team in early 2015. City and Hunslet Ward Members were consulted by email on 24 March 2014 at pre-application stage.
- 5.2 The Ruth Gorse Academy and their professional team presented their initial scheme to City Plans Panel on 16 April 2015, and Members made the following comments:
- the length of time a new high school for South Leeds had been discussed
 - the need for a clear mechanism for community use of the facilities to be established
 - the need to balance the safeguarding of pupils with providing connectivity through the site, possibly achievable through the proposed car park
 - the challenging timescales being proposed and that Panel, whilst recognising the need of a new school would not sanction a development which was not satisfactory
 - the design of the building, with concerns it did not make a strong enough statement, particularly in comparison to the Leeds College of Building; that the brick element was uninspiring; that the extent of the dark cladding to the community hub element appeared to 'push down on' and visually dominate the ground floor glazed elements
 - the level of car parking being proposed; that this was not sufficient; that car sharing would be difficult to insist upon and that additional car parking would be required off-site

- the access arrangements for the car park; the pickup and drop off points and how the pupil spill out areas would work
- the noise levels around the site due to the traffic along Black Bull Street; the need for traffic calming measures, with a 30mph limit being suggested and that acoustic fencing may be required
- highways issues and the need for the traffic lanes at Black Bull Street to be reduced before pupils were on site
- the need to consider the type of internal flooring materials to specialist classrooms to ensure this provided a level of comfort for teaching staff who had to stand for long periods of the day
- Members were informed that funding for the project was limited and that the applicants required the largest area of expenditure to be used where teaching and learning would take place. That requirements relating to daylight levels had to be satisfied, which affected the design of the building. Whilst the nearby Leeds College of Building had limited glazing on the workshop elements, this would not be possible for the academy. Similarly the funding streams differed between colleges and academies, with colleges having greater autonomy. The Chair noted these points, but summed up the view of the Panel that improvements could be made to the design of the building

In relation to the specific points raised in the report, Members provided the following responses:

- that the proposed use of the site would be appropriate in principle
- to note the qualified comments in respect of the form, massing, architectural treatment and materials in respect of the regeneration aspirations for the area
- in relation to the boundary treatment, concerns were raised about the proposed paladin fencing to the boundaries; that such fencing was easily vandalised and that an improved form of boundary treatment was required and that more screening should be provided to Black Bull Street to help mitigate against noise levels
- that it was necessary to secure a pedestrian and cycle connection through the site in order to enhance pedestrian connectivity between the South Bank and the rest of the City Centre
- to note Members' comments in respect of the highways and transportation issues

6.0 PUBLIC/LOCAL RESPONSE:

6.1 Planning application publicity consisted of:

6.1.1 Site Notice posted 22.05.2015

6.1.2 Press Notice published 28.05.2015

6.1.3 City and Hunslet Ward Councillors consulted by email 27.05.2015

6.2 Leeds Civic Trust supports the application proposal and has made the following comments:

The Trust is fully in accord with the aspirations of the Ruth Gorse Trust and development on the site selected – here it will contribute to the regeneration of the South Bank, help establish a sub-regionally significant education campus and deliver excellent education to the more deprived communities within the city centre rim. We also note and agree with the applicant's site analysis and how this translates into a workable model for both school and community use of the premises. However, notwithstanding their support of the principles, the Trust does have some observations on the plans as submitted.

1. Although many aspects of this will be outside the direct control of the Academy, it is essential that the building is linked into both existing and emerging patterns of pedestrian, cycle and vehicular movement in South Bank, and linking out to the wider communities it will serve. There is a working group looking at transport options for South Bank and we feel the Academy should be represented on this. The Academy site will be a 'blockage' for east-west movements and every effort should be made to minimise its impact by making the secure area as small as possible. We welcome the recent changes made to the northern end of the site to provide better access to the area allocated for the NGT stop.
2. The level of traffic on Black Bull Street has had an impact on the design of the buildings but there is potential for the use of this route to be reduced significantly in future by
 - narrowing the carriageway (as is proposed and we would support)
 - creating pavement-level crossings to slow traffic
 - introducing a 20 mph school speed limit
 - closing the link between St Peter Street and Crown Point Bridge (outside the Ibis Hotel) so all traffic is diverted to the north and east of the Gateway site – through traffic would be encouraged to use East Street as a route to the motorway and traffic for Black Bull Street would need to make a more deliberate backtrack.
3. We welcome the commitment to 'park & stride' to prevent too many pupils being dropped off around the school and we look forward to seeing how this works. With regard to on-site parking, we feel that the 'future car park expansion space' shown on the site plan should not form part of the current planning application – every effort should be made to reduce day-to-day car use and there are other public car parks in the vicinity for occasional use. The space should be fully landscaped to help ensure a high quality environment for the future NGT stop – we feel that, in partnership with the WYCA, an interim landscaping scheme should be implemented for the land intended for long term use by NGT vehicles.
4. With regard to cycling, it was disappointing to see the applicant's lack of knowledge of the plans for Hunslet Stray (which will provide a direct route to Leeds Bridge) and of the Council's significant investment in the CityConnect cycle superhighway and towpath enhancements which have the potential to deliver pupils (and staff) safely to school, largely on off-carriageway routes. To that end, provision should be made for many more cycle parking spaces in secure locations which can be monitored by both passers-by and CCTV – the Academy could have a major aspiration to raise cycle usage to more than the existing standards.
5. We feel the scale and design of the buildings is appropriate for the location although we have some concerns over the 'maintainability' of recessed windows, white render and other light-coloured surfaces in what will remain a dusty environment for some years.
6. We welcome the commitment to public access to the facilities within the building and hope that due consideration will be given to the practicability of such an operation as the design develops:
 - lockdown doors to minimise areas of the school to which there would be out-of-hours access
 - no hidden corners and corridors where people can hide from view

- provision for a 'reception desk' where users can pay for activities
- additional sports hall storage for adult-standard equipment – doors to this should open outwards for ease of access (and stretch across as much of the available width as possible)
- clear routes from the changing area to the outdoor pitches and the studios
- full provision of accessible change/WC/shower rooms.

7. We welcome changes to the proposed boundary treatment to Chadwick Street since our presentation – we feel the high quality brick and railing solution is far more appropriate for what will be a key elevation in the long term.

7.0 CONSULTATIONS RESPONSES:

7.1 Statutory:

7.1.1 LCC Transport Development Services

The proposals are acceptable in principle - discussions are on-going with the applicant regarding a number of issues including:

- pupil drop off/pick up
- off-site highways works including the widening of footways on Chadwick Street
- A vehicular access is proposed from Black Bull Street to the car park / service area, the layout proposed on the drawings is acceptable.
- A secondary access is proposed on Chadwick Street for maintenance and occasional use, this will be at the location of an existing wide access point. The existing access should be reinstated as footway with a dropped kerb crossing that affords pedestrian priority along the footway.
- The car park area provides a suitable manageable area for deliveries and refuse collection.
- Whilst the proposed level of cycle parking has been accepted as a starting point, space has been identified for expansion of the facilities in the future by a further 30 spaces. This should be covered by condition
- Staff who need to have a predominantly car borne journey will be to utilise park and ride. A site on the A63 in the Aire Valley will open next year with stops on Hunslet Road in close proximity to the site, this will provide a good opportunity and should be highlighted more in the Travel Plan.
- Off-site highway works have been identified to narrow Black Bull Street to two running Lanes and provide a toucan crossing on a buildout and bus stop alterations. WYCA have been asked to comment on the bus stop alterations. As described above the crossing should be slid southwards to assist in planning for a second crossing on the future.
- In addition to the above, the existing redundant vehicular access points on Black Bull Street and Chadwick Street must be reinstated as footway, the vehicular access is required from Chadwick Street for maintenance purposes, this should be constructed as a dropped crossing.
- As described above in the accessibility section, alterations to Traffic Regulation orders will be required on Chadwick Street to relocate car parking to provide safe crossing points and sightlines to the secondary access, the crossings should be identifiable with dropped Kerbs and tactile paving.
- The car park management plan and servicing plan referred to in the TA should be conditioned.

Comments are awaited from Highways officers regarding the latest submitted off-site highways works plans, updated travel plan, and construction management plan. These matters will be updated verbally at the Panel meeting.

7.1.2 Environment Agency
No objection subject to the implementation of the recommendations of the submitted Flood Risk Assessment

7.2 Non-statutory:

7.2.1 LCC Flood Risk Management
No objection

7.2.3 LCC Environmental Protection & Air Quality Management Team
No objection

7.2.4 Yorkshire Water
Yorkshire Water initially advised that the proposed surface water drainage strategy is not acceptable as submitted (relates to run off rates from the site). The applicants are in discussions with YW at the time of writing.

7.2.5 LCC Forward Planning and Implementation
No objection to the proposed use. The flood risk sequential and exceptions tests have been passed.

7.2.6 West Yorkshire Combined Authority
WYCA made the following comments:

- The TA provides a comprehensive appraisal of the public transport accessibility of the site and highlights a number of public transport services that are located within the vicinity of the development. The general location of the site on the edge of the city centre inevitably means that the site is, at a strategic level, an accessible site.
- The walking environment is not ideal. As Black Bull Street is only one way, public transport users will have to cross Black Bull Street to access northbound services on Crown Point Road to or from the school. WYCA note that a pedestrian crossing will be provided which is supported.
- The proposals include the creation of additional bus laybys and the relocation of the existing bus stop (shelter) on Black Bull Street by converting the nearside lane into a bus drop off area. The existing public service stop (45011975) is proposed to be relocated north of its current position, north of the proposed school service stops. It is suggested that these bus stop locations should not be fixed as part of the application. Further discussions with the bus operators would be needed to get their preference on the final stop locations.
- A bus shelter with real time passenger information would be required at the relocated service bus stop (£20,000).
- With regard to the school bus provision, it is not clear what the school service provision is likely to be. The TA states that school bus services are likely to be required yet the application doesn't appear to have any firm commitments to quantify how many school buses will be required. The 2 bus bays for school services may not be adequate provision.
- It should also be noted that the TA indicates that the public service bus stops could be used for coaches / school services on Black Bull Street and Chadwick Street. This is not an acceptable strategy. School services and coaches must not be permitted to lay over at regular service bus stops.
- The site will benefit from the NGT scheme as this scheme is developed, and a temporary route across the site would be in principle subject to an Agreement with Ruth Gorse which could include:
 - o implementing a temporary, fit for purpose and clear access across the NGT site, which would be Ruth Gorses responsibility to maintain until implementation of NGT

- The Academy to be responsible for their own boundaries and security.
- Once NGT is under construction in this section it will be required for NGT but the design will allow for access into Ruth Gorse, subject to the NGT stop design.

7.2.7 LCC Land Contamination

The submitted documents including remediation strategy are acceptable. The development shall be carried out in accordance with the agreed remediation strategy. A condition will require verification of the works and the applicant will need to demonstrate that the site is suitable for use prior to occupation.

7.2.8 LCC Minerals

The Coal Recovery report is acceptable and meets the requirement of NRWDPD policy MINERALS 3.

8.0 PLANNING POLICIES:

8.1 Development Plan

The Leeds Core Strategy was adopted by the Council on 12th November 2014. This now forms the development plan for Leeds together with the Natural Resources & Waste Plan and saved policies from the Leeds Unitary Development Plan Review 2006 (UDPR). Relevant Saved UDPR Policies would include

GP5 all relevant planning considerations

BD2 new buildings

T7A cycle parking

T7B motorcycle parking

T24 Car parking provision

LD1 landscaping

Relevant Core Strategy Policies include:

Spatial Policy 3 sets out the role of Leeds City Centre, including the comprehensive planning of redevelopment and re-use of vacant and under-used sites and buildings for mixed use development and new areas of public space. It also states that new development shall enhance streets and create a network of open and greenspaces to make the City Centre more attractive, family friendly and easier for people to use, and in consolidating and enhancing sense of place

Spatial Policy 4 identifies the Aire Valley Leeds as a Regeneration Priority Programme Area. Priority will be given to developments that include housing quality, affordability and choice, improve access to employment and skills development, enhance green infrastructure and greenspace, upgrade the local business environment and improve local facilities and services.

Spatial Policy 5 sets out the broad principles for development in the Aire Valley Regeneration Priority Programme Area including targets for housing (6,500 units) and employment land (250 ha) specific to the area.

Spatial Policy 8 states that training/skills and job creation initiatives would be supported by planning agreements linked to the implementation of appropriate developments given planning permission.

Spatial Policy 11 Transport Investment Priorities – includes a priority related to improved facilities for pedestrians to promote safety and accessibility, particularly connectivity between the edges of the City Centre and the City Centre itself.

Policies CC2 and CC3: Improving connectivity between the north and south parts of the City Centre and neighbouring communities – provide and improve routes connecting the City Centre with adjoining neighbourhoods to improve access and make walking and cycling easier. Policy CC2 makes specific reference to the creation of a new east-west links to Leeds Dock. It states that large scale edge of centre development which would prejudice the achievement of this priority will be resisted.

Policy G9 Biodiversity improvements

Policies EN1 & EN2 set targets for CO2 reduction and sustainable design & construction, including BREEAM Excellent and at least 10% low or zero carbon energy production on-site.

Policy EN5 – flood risk. A flood risk assessment and sequential test would be required as the site lies in Flood Zone 3.

Policy P9 sets out the policy position for new community facilities including a requirement that they should be accessible by foot, cycling, or by public transport.

Policy P10 requires new development to be based on a thorough contextual analysis to provide good design appropriate to its scale and function, delivering high quality innovative design and enhancing existing landscapes and spaces.

Policy P12 states that landscapes will be conserved and enhanced.

Policies T1 and T2 identify transport management and accessibility requirements for new development.

Natural Resources & Waste Plan

The Natural Resources and Waste Development Plan Document (Local Plan) is part of the Local Development Framework. The plan sets out where land is needed to enable the City to manage resources, like minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way. Policies regarding flood risk, drainage, air quality, trees, and land contamination are relevant to this proposal. The site is within the Minerals Safeguarding Area for Coal (Minerals 3).

Emerging Aire Valley Area Action Plan

The site is located within the Aire Valley Leeds Regeneration Programme Area for which an Area Action Plan has been drafted and agreed for public consultation. This seeks an east-west connection across the centre of this site between Black Bull Street and Leeds Dock as part of a network of new greened pedestrian and cycle connections. This will form part of the development plan when adopted and make allocations within the area (replacing or renewing existing UDP land use allocations). It has limited weight in decision making currently but signals the Council's aspirations and priorities for the future development of the area.

Relevant Supplementary Planning Guidance includes:

SPD Street Design Guide

SPD Travel Plans

SPD Building for Tomorrow Today: Sustainable Design and Construction

SPG City Centre Urban Design Strategy

South Bank Planning Statement

The site lies adjacent to the area covered by the South Bank Planning Statement (see attached Plan 1 South Bank Urban Design Principles). A key principle for the South Bank is that new enhanced landscaped connections would be formed across the wider South Bank area, eventually linking Holbeck Urban Village and the City Centre Core (including the railway station and central bus stops west of the bus station) to Leeds Dock via the City Centre Park. All new developments on sites adjacent to Crown Point Road, Black Bull Street and Hunslet Road/Lane would need to deliver the continuation of landscaped pedestrian and cycle routes across the wider area, and the necessary pedestrian connectivity improvements such as crossings to major roads like Crown Point Road and Black Bull Street, other upgraded crossing points, increased pavement widths and lane reductions, in order to make new development acceptable. This proposal has the potential to contribute towards a coordinated series of key pedestrian connectivity improvements as identified in the adopted South Bank Planning Statement. This would start from the City Centre core, via the Riverside and Bridge End, through the former Tetley Brewery site via a re-opened Hunslet Road (which has been secured via a Section 106 agreement attached to the Carlsberg temporary car park permission ref. 11/05031/FU – this route would also be retained as a link in the future City Centre Park in any permanent redevelopment), across Crown Point Road via the currently under construction pedestrian crossing, along the new pedestrian/cycle path through the Leeds College of Building site, then across Black Bull Street, linking to retail, leisure, food and drink facilities at Leeds Dock.

National Planning Policy Framework

The National Planning Policy Framework (NPPF) represents the government's commitment to sustainable development, through its intention to make the planning system more streamlined, localised and less restrictive. It aims to do this by reducing regulatory burdens and by placing sustainability at the heart of development process. The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England and how these are expected to be applied, only to the extent that it is relevant, proportionate and necessary to do so.

The NPPF identifies 12 core planning principles (para 17) which include that planning should:

- Seek high quality design
- Actively manage patterns of growth to make the fullest possible use of public transport, walking and cycling.
- Planning should proactively support sustainable economic development and seek to secure high quality design. It encourages the effective use of land and achieves standards of amenity for all existing and future occupiers of land and buildings. One of the core principles is the reuse of land that has previously been developed.

Paragraph 72 attaches great weight to the need to create, expand or alter schools and advises the following:

- The Government attaches great importance to ensuring that a sufficient choice of school places is available to meet the needs of existing and new communities. Local planning authorities should take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education. They should:
 - Give great weight to the need to create, expand or alter schools; and

- Work with schools promoters to identify and resolve key planning issues before applications are submitted.

In assessing school developments the decision maker must also be mindful of a policy statement issued jointly by the Secretary of State for Education and the Secretary of State for Communities and Local Government on the 15th August 2011. This sets out the Government's commitment to support the development of state-funded schools and their delivery through the planning system. It states that the Government is firmly committed to ensuring there is sufficient provision to meet growing demand for state-funded school places, increasing choice and opportunity in state-funded education and raising educational standards. It goes on to say that the Government believes that the planning system should operate in a positive manner when dealing with proposals for the creation, expansion and alteration of state-funded schools, and that the following principles should apply with immediate effect:

- i) There should be a presumption in favour of the development of state-funded schools, as expressed in the National Planning Policy Framework.
- ii) Local authorities should give full and thorough consideration to the importance of enabling the development of state-funded schools in their planning decisions. The Secretary of State will attach significant weight to the need to establish and develop state-funded schools when determining applications and appeals that come before him for decision.

9.0 MAIN ISSUES

- 9.1 Principle of use
- 9.2 Design
- 9.3 Landscaping and connectivity
- 9.4 Amenity
- 9.5 Sustainability
- 9.6 Flood risk
- 9.7 Highways and transportation
- 9.8 Planning obligations

10.0 APPRAISAL

10.1 Principle of use

- 10.1.1 It is considered that the principle of the proposed use is appropriate to the vision for the regeneration of the South Bank and the Aire Valley, and would be acceptable in the context of the NPPF and the Core Strategy. It is noted that residential development forms part of the regeneration plans for the City Centre, South Bank and Aire Valley, and this will bring with it a need for new community facilities, such as schools, to serve the growing population. This is reinforced by the housing targets set out in the Core Strategy for the City Centre (10,200 in Spatial Policy 7) and Aire Valley Leeds area (6,500 in Spatial Policy 5) which will necessitate the need for additional/extended schools in the area. It is considered that the provision of a secondary school at this site would act as a catalyst for the regeneration of the surrounding area, and encourage family housing provision in the City Centre and Aire Valley urban eco-settlement. The vision for this part of the City Centre is for a mixed use environment with no dominant single land use. This is to encourage a range of residential, business and community uses that create activity at different times of the day and all year round. Recent developments in the area include new further

education facilities for Leeds City College and Leeds College of Building, and the future University Technical College in part of the Braimes Pressings factory (subject of a current planning application), but taking account of the wider mix of land uses and available development sites in the area it is considered that educational use on this site would not have an adverse impact on the emerging character of the area.

10.2 Design

10.2.1 The South Bank will form an extension of the City Centre core, with a broad mix of uses and facilities, making an attractive, well connected 'place' for 21st Century Leeds. The provision of a secondary school is an important facility which would assist in meeting these aims, however the integration of such a use in a City Centre environment needs careful consideration in place-making terms. The surrounding area features a range of modern commercial and residential buildings such as large scale 8 storey modern buildings at Leeds Dock and Brewery Wharf, and lower scale uses such as warehouse, car showrooms and light industry. It also features listed and unlisted industrial heritage assets such as Chadwick Lodge (Grade II*), the former Alf Cooke Printworks (Grade II), and the Malthouse on Chadwick Street. In ranging between three and four storeys, with a mixture of red-brick, glazing, and metal cladding, it is considered that on balance the scheme features appropriate form, massing, architectural treatment and materials that respects the setting and scale of adjacent and nearby buildings.

10.2.2 The applicant has responded positively to Member comments at pre-application stage, and a number of changes to the architecture of the building have been made. Members had concerns regarding the design of the The Hub Block (sports block). Its height has now reduced and the 'kick-up' of the roof line omitted to reduce the overall massing and weight of block. The Hub Block structure has been redesigned to cantilever at first floor level to Black Bull Street which would allow the omission of the columns below the facade projection to give the appearance of the block floating over its plinth. The increased amount of full height glazing to the LRC would provide active frontage to the street. A lighter profile detail between the Hub Block cladding base and the plinth to further reduce the overall height of the cladding create an appropriate transition between the two elements. The proposed cladding panels have been changed to a higher specification metal panel with smoother finish, laid in vertical sections, with hidden fixings. The strong vertical window reveals to the Hub blocks would be recessed to 190mm. The proposed external walkway to the north elevation is not considered to be a desirable feature; however it is necessary to provide emergency escape for a large number of students and staff from the gym and main hall facilities at first floor level. The external detailing of the walkway has been revised to match the detailing and cladding of the main block, with dark grey metal balustrading and a lighter channel detail along the edge of the deck. The walkway is set well into the site from Black Bull Street, with car parking, soft landscaping and the NGT stop between it and the main areas of public highway it would be viewed from. Railings and soft landscaping immediately to the east of 48-50 Black Bull Street would improve the visual appearance of the northern boundary to Chadwick Street. The main entrance has been reconfigured to provide a full height glazed entrance screen in one plane, to a forming a clear glazed separation between the Hub and STEM blocks, which would also contribute positively to the streetscene.

10.2.3 The STEM block would front the remainder of Black Bull Street, featuring deep 175mm window reveals to the red brick façade. On the Black Bull Street elevation, the upper floor windows have been grouped into a singled glazed unit, allowing the recessed window elements to sit in-plane as fully glazed elements, by omitting the infill cladding. A 'saw tooth' roof profile to the STEM block has been introduced,

which would break up the building's roof line and responds to the views from Crown Point Bridge and Hunslet Road. The element also houses some of the buildings photovoltaic panels, making one of the buildings low carbon / sustainable features visible. The southern gable to the STEM block has also been revised, with the glazed slot to the STEM wing being recessed further into the facade to create relief and providing a clear break between the two masonry blocks.

10.3.4 To the Humanities block the blue/grey brick plinth would have approximately 140mm deep window reveals with approximately 160mm window reveals to the render above. The render material to the Humanities block is on balance considered acceptable. Render may be prone to an untidy appearance due to weathering, however if well maintained well, can be an appropriate material. Render buildings are found at Leeds Dock, and have weathered better than other more recent examples. Provided the Academy maintains the building by cleaning and repainting, the proposed materials to the Humanities block are considered on balance acceptable given the overall positive architectural treatment of the three main building elements.

10.3.5 Overall, the proposed changes are considered to be an improvement on the original pre-application design, and result in a design that would enhance the character of the Black Bull Street and Chadwick Street frontages. The proposal would integrate well into the emerging character of the area, and complement the Leeds College of Building, Leeds Dock, and the listed former Alf Cooke Printworks and Chadwick Lodge.

10.2.6 In considering how the Academy would integrate into a City Centre street context, the treatment of the site boundaries is particularly important. The building edge forming a boundary to Black Bull Street, including street tree planting is considered appropriate in an emerging City Centre context. The applicant has reconsidered the boundary treatments around the site and these are now considered to be appropriate to the character of the surrounding area. The secure boundary to the proposed NGT stop has been adjusted, moving the fence back and aligning the secure line with the escape walkway from the Hub block. This has opened up the site and greatly reduced the extent of fencing to the northern end of the site, whilst not compromising the schools secure line. Paladin fencing has been omitted from the scheme with the southern boundary now enclosed with railings. The paladin fence to the north boundary has been replaced with masonry piers and railings to match those areas of retained and modified boundary treatment. Brick piers have been also added to the corners of the MUGA fencing. The MUGA fencing is set back into the site, and would be concealed to some extent by the car showroom boundary fence, and the soft landscaping and trees to the Black Bull Street frontage, and therefore it is considered that visually this would be acceptable. The existing high brick wall to Chadwick Street is proposed to be retained. Given the security and safeguarding concerns that the school has, it is considered that the form and height of these boundary treatments are suitable for a City Centre environment. New tree planting to Black Bull Street and Chadwick Street would go towards meeting the aspirations for tree-lined and landscaped road frontages in the South Bank Planning Statement, and the overall planting scheme would help to soften the urban environment and promote biodiversity. However the constraints of the competing space demands of different school functions on the site, and the applicant's daylight requirements for classrooms, means that less street frontage planting has been proposed than would be desirable along Black Bull Street and Chadwick Street. On balance, when weighed against the overall benefits of the scheme, this more limited provision of soft landscaping is considered acceptable.

10.3 Pedestrian and cycle connectivity

- 10.3.1 The school would have a duty of care for its pupils and this requires that a secure boundary treatment is provided around the site to ensure students are safeguarded. Given the level of accommodation and facilities proposed at this site, a public route through the centre of the site to link from the proposed new crossing on Black Bull Street to Leeds Dock cannot be achieved. The Academy has stated that they would offset this lack of pedestrian connection with community use of facilities, which would help integrate the school into the community. Whilst this is welcomed as a community facility, it would not meet the place-making and connectivity aspirations of the South Bank Planning Statement, and the requirements of Core Strategy Policies P10, SP3, CC2 and CC3. It is considered that the creation of a large impermeable development without taking the opportunity to provide and continue the emerging network of pedestrian connections east to west through the South Bank would potentially undermine the successful regeneration of this area. It is considered that the proposed east-west connection through the site does not achieve the quality of pedestrian permeability sought by the Council's planning policies. Since the pre-application presentation the proposed link at the northern end of the site has been moved south further into site with potential for improved onward connections with NGT and Leeds Dock beyond. The proposed link has also increased to 3m wide with landscaped edges. However the safeguarding issues raised by the introduction of a school community into this location are recognised and the school's willingness to provide limited accessibility through the northern part of the site and the proposal for achieving potential future connectivity along the southern boundary are noted. With reference to paragraph 72 of the NPPF, which states that local planning authorities are to *"give great weight to the need to create, expand or alter schools"*, it is therefore considered that on balance the benefits that the development would bring to this area would outweigh the limited provision for improvements to east-west pedestrian and cycle connectivity across the site.
- 10.3.2 The proposed development provides the opportunity for a future east / west link to be constructed to the southern boundary of the application site. It would be expected that the completed route would be delivered in conjunction with adjacent development proposals if and when this occurs in the future. This would be based on the likely costs for laying out the space and provision of appropriate surfacing, lighting and other work requirements. The Academy secure boundary line is set 1.5m away from the existing boundary walls (in 3rd party ownership) across the southern edge of the site. This 1.5m wide zone has been created to avoid the risk of damage to the 3rd party walls and to allow maintenance access. This zone would be gravel finished for low maintenance and ease of construction of a future 3m wide permanent paved link. The Academy has agreed to contribute this area of land to facilitate the future link, when the land to the south of the Academy comes forward for development. Officers have advised that the Academy will be required to contribute towards the future surfacing and lighting of the route in combination with the neighbouring landowners.
- 10.3.3 Officers have also requested that an interim landscaping scheme be developed by the Academy to link to Chadwick Street prior to the delivery of new public realm at the NGT stop. The applicant would provide a fully finished east/west footpath through the car park within the application site demise as shown on the site masterplan. A temporary continuation across WYCA land to provide a completed east / west connection would need to be provided pending completion of the proposed NGT stop works. The Academy is unable to provide this footpath continuation because the land is not within the applicant's control, that needs to a section of boundary wall removed, needs to be decontaminated, drained and surfaced and enclosed with fencing, and there is also a large stockpile of potentially contaminated material on the land.

10.4 Amenity

10.4.1 It is considered that in the context of a busy mixed-use former industrial City Centre environment, the proposed school use would not result in significant adverse impact on the amenities of nearby residents at Leeds Dock. It is considered that the buildings would be sited sufficiently far away to avoid overlooking and loss of privacy to existing residents at Leeds Dock on the opposite side of Chadwick Street. It is considered that the appropriate regeneration of this long vacant site would enhance the amenities of the surrounding area compared to a cleared rough surfaced site.

10.5 Sustainability

10.5.1 The application has been assessed against the requirements set out in Core Strategy Policy EN1 (carbon dioxide reduction) and EN2 (sustainable design). It is noted that the building is likely to achieve a high BREEAM Very Good rating which falls marginally below the Core Strategy BREEAM Excellent standard set by Core Strategy Policy EN2. However, this should be weighed against the positive sustainability credentials of the proposals, such as exceeding the 20% reduction in carbon emissions compared to Building Regulations (25%), its overall energy performance (which would meet exceed the mandatory level for BREEAM Excellent) and renewable energy generation by solar PV panels (which would also exceed the level required by BREEAM Excellent), its accessible location, the re-use of a brownfield site, and wider contribution to the surrounding area.

10.6 Flood risk

10.6.1 The site is located within Flood Zone 3 according to the Environment Agency's Flood Risk Map. In accordance with NPPF the applicant has undertaken a flood risk sequential assessment. This has assessed potential alternative sites within the Aire Valley Leeds AAP area. The applicant has assessed proposed mixed use sites from the emerging draft AAP document (as agreed for consultation by Executive Board in February 2015) and found that none of the sites situated in a lower flood risk zone is of sufficient size and/or available immediately to accommodate the application scheme. It is considered that this approach is robust and that the sequential test has been satisfied. According to the flood risk vulnerability classification in Table 2 of the NPPG advice on flood risk, educational uses are classified as 'more vulnerable' and therefore a flood risk exception test is also required in accordance with Table 3 of the NPPG. The applicant has undertaken an exception test and it is considered that the sustainability benefits of the proposals have been demonstrated and that this outweighs flood risk as required by part (i) of the test set out in NPPF para 102. Part (ii) of the test relates to the safety of the development and the Council's Flood Risk Management team have confirmed their agreement on these matters. The site is considered sustainable given its location on a previously developed City Centre brownfield site, within an identified regeneration area, built to high sustainability standards, accessible to pedestrians and cyclists and close to public transport links. These wider sustainability benefits are therefore considered to outweigh potential flood risk matters in this case, subject to the implementation of the recommendations of the agreed flood risk assessment.

10.7 Highways and transportation

10.7.1 Since the pre-application presentation a number of changes to the scheme have taken place in response to Member and Officer comments:

- alterations to white lining on Black Bull Street to tie into existing, remove the deceleration lane and improve cycle lane provision
- bus stops on Black Bull Street moved slightly south to allow more space for service buses (to be subject of further negotiation with WYCA)
- dropped kerbs and tactile paving at car park entrance shown on plans
- on Black Bull Street the proposed signalised pedestrian crossing has moved further south to link path across the north of the Leeds College of Building and the Academy student entrance. The crossing has been widened to 5m to provide increased capacity for children crossings
- access plans amended to show proposals for Chadwick Street including footway extension into the site on Chadwick Street and informal crossing point to link to Leeds Dock
- car park capacity has been increased from 32 to 41 spaces.
- cycle parking provision increased with further locations identified for future additional storage to be introduced.
- an access barrier has been introduced to car park with queuing space on the highway side to prevent unauthorised parking or drop off in the car park.

10.7.2 The Saved UDPR maximum parking standard for a school is 1 space per 2 staff plus visitor parking. At this site, given its sustainable location, good access to public transport, walking and cycling alternatives, a car park management plan controlled by condition, and the provision of a robust travel plan, it is considered that a significantly lower parking provision would be acceptable. Parking in the immediate surrounding area is controlled and enforced, and members of staff that decide to drive to work would be able to park nearby at Leeds Dock where there is a circa 1600 space multi-storey car park with sufficient capacity.

10.7.3 It is considered that the scheme would feature appropriate levels of car, cycle and public transport provision. The catchment for the school is generally low car ownership and higher bus usage areas of the city. Under Core Strategy P9, the location of the site within the city centre and close to existing bus stops and services and a proposed stop on the NGT trolleybus network provides an accessible location which should encourage journeys (both students and staff) by non-car modes of travel. The site lies within walking distance from the City Station, the bus station and bus services/stops along Chadwick Street (university bus), Black Bull Street, Hunslet Road and the City Centre core. Recent sustainable travel improvements for the South Bank include a water taxi linking Granary Wharf and Leeds Dock, and a new South Bank shuttle bus from Boar Lane. The Aire Valley and Elland Road Park and Rides would benefit the site, and NGT would serve the site from the north and south in the future if approved.

10.7.4 The scheme includes an updated Travel Plan, which includes measures to ensure low car usage for staff, pupil pick up and drop off, and “park and stride” initiatives. This is currently under assessment and progress will be updated verbally at Panel.

10.7.5 The provision of a new pedestrian crossing on Black Bull Street would improve pedestrian connectivity from the city centre core via the former Tetley Brewery site and the new crossing on Crown Point Road delivered by the Leeds College of Building scheme. Bus drop off–lay-bys and footway build-outs would reduce the width of Black Bull Street from three lanes of traffic to two lanes of traffic. Other off-site highways works would include amendments to traffic regulation orders (TROs) on Chadwick Street, creation of new bus lay-bys to Black Bull Street, and changes to lining and signing on Chadwick Street and Black Bull Street. These matters would be controlled by condition, and provided prior to the first occupation of the school.

10.7.6 The comments made by WYCA on 13 July 2015 will be responded to verbally at Panel. The applicant has responded to two of the points. Firstly, their current understanding is that there will be a service, but pupils will have to pay for it. Details of routes, numbers of pupils using the services, numbers of buses, and timings have not been determined at this stage, particularly as the home locations of many of the pupils who will attend in 2016/17 are currently unknown. School bus services would be implemented as part of the Travel Plan, therefore details would become available by the end of Summer Term 2016. With regards to coach parking, the TA states that *“For school trips and other outings, buses will be able to pick up and drop off from the proposed Black Bull Street [specific school bus bays] bus stops or Chadwick Street, depending on timings, traffic conditions and the number of buses. This is only an occasional event so will be managed by TRGA to minimise disruption to traffic.”* Coaches would be managed by staff and use the specific school bus stops on Black Bull Street (subject to their not being in use by dedicated school services at that time) and not the public service bus stops.

10.8 Planning obligations

10.8.1 As part of Central Government’s move to streamlining the planning obligation process it has introduced the Community Infrastructure Levy Regulations 2010. This requires that all matters to be resolved by a Section 106 planning obligation have to pass 3 statutory tests. The relevant tests are set out in regulation 122 of the Regulations and are as follows:

‘122(2) A planning obligation may only constitute a reason for granting planning permission for the development if the obligation is-

- necessary to make the development acceptable in planning terms;
- directly related to the development; and
- fairly and reasonably related in scale and kind to the development.’

The Council’s adopted policies would result in the following Section 106 matters, which are considered to be necessary, directly related to the development and fairly and reasonably related in scale and kind to the development:

- Travel plan monitoring fee £2,500
- Community use of facilities
- Public route across the site east to west at the northern end of the site
- Contribution towards the delivery of an east-west route at the southern end of the Site - sum to be agreed
- Local bus stop improvement £20,000
- Cooperation with local jobs and skills initiatives
- Management fee £1,500

10.8.2 The proposed development is not liable for the Community Infrastructure Levy (CIL) because the applicant’s agent has confirmed that it is a publicly funded non-profit making scheme.

11.0 CONCLUSION

11.1 In response to other comments made in representations by Leeds Civic Trust that have not been discussed above, regarding the community use of facilities, the Ruth Gorse Academy will work with other education establishments and wider community to maximise the use of their facilities. This is outlined in the document submitted with the application. It is envisaged that the detailed arrangements will be set out in a community use agreement which would be secured via the Section 106 agreement.

The comments raised by Leeds Civic Trust have been passed to the Academy and discussed in more detail during the s106 negotiations.

- 11.2 The above matters are considered to be the main planning issues. As a publically funded school proposal, paragraph 72 of the NPPF is relevant. This requires local planning authorities to “give great weight to the need to create, expand or alter schools”. All other matters raised by consultees have been assessed and are not considered to outweigh the conclusion that on balance, the proposals are considered to comply with the Council’s substantive adopted policies, and would constitute acceptable sustainable development. The proposal would lead to the delivery of a new educational facility for the City Centre and inner south Leeds within the South Bank and Aire Valley regeneration area, and deliver the regeneration of a longstanding cleared brownfield City Centre site in a sustainable location.

Background Papers:

Application file 15/02470/FU

Appendix 1 Draft conditions

Appendix 2 South Bank Planning Statement Urban Design Principles Plan 1

Appendix 1 - Draft Conditions for application reference 15/02470/FU

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans Schedule.

For the avoidance of doubt and in the interests of proper planning.

- 3) No construction of external walling or roofing shall take place until details and samples of all external walling and roofing materials have been submitted to and approved in writing by the Local Planning Authority. Such materials shall be made available on site prior to the commencement of their use, for the inspection of the Local Planning Authority who shall be notified in writing of their availability. This shall include a large sample panel of all external facing materials and glazing types to be used. The external cladding and glazing materials shall be constructed in strict accordance with the sample panel(s) which shall not be demolished prior to the completion of the development.

In the interests of visual amenity in order to accord with Leeds UDP Review Policies GP5 and BD2, Leeds Core Strategy Policy P10 and the NPPF.

- 4) Notwithstanding details shown on the plans hereby approved, no external walling or roofing shall be constructed until typical 1:20/1:50 scale elevations/section/plan working drawings showing the following details junctions between materials including rooftop parapets, window and door openings and all junctions of materials have been submitted to and approved in writing by the Local Planning Authority. Works shall be carried out in accordance with the details thereby approved and retained as such thereafter.

In the interests of visual amenity in order to accord with Leeds UDP Review Policies GP5 and BD2, Leeds Core Strategy Policy P10 and the NPPF.

- 6) No external surfacing works shall take place until details and samples of all external surfacing materials have been submitted to and approved in writing by the Local Planning Authority. Such materials shall be made available on site prior to the commencement of their use, for the inspection of the Local Planning Authority who shall be notified in writing of their availability. The surfacing works shall be constructed from the materials thereby approved prior to occupation of the building.

In the interests of visual amenity, in accordance with Leeds UDPR Policies GP5 and LD1, Leeds Core Strategy Policy P10 and the NPPF

- 7) External surfacing or landscaping works shall not commence until full details of both hard and soft landscape works including an implementation programme have been submitted to and approved in writing by the Local Planning Authority. Hard landscape works shall include
 - (a) proposed finished levels and/or contours,
 - (b) boundary details and means of enclosure,
 - (c) car parking layouts,

- (d) other vehicle and pedestrian access and circulation areas,
- (e) hard surfacing areas,
- (f) minor artefacts and structures (e.g. furniture, play equipment, refuse or other storage units, signs, lighting etc.),
- (g) proposed and existing functional services above and below ground (e.g. drainage, power cables, communication cables, pipelines etc., indicating lines, manholes, supports etc.).

(h) lighting

Soft landscape works shall include

- (i) planting plans
- (j) written specifications (including soil depths, cultivation and other operations associated with plant and grass establishment) and
- k) schedules of plants noting species, planting sizes and proposed numbers/densities.
- l) tree pits and soil volumes

All hard and soft landscaping works shall be carried out in accordance with the approved details, approved implementation programme and British Standard BS 4428:1989 Code of Practice for General Landscape Operations. The developer shall complete the approved landscaping works and confirm this in writing to the Local Planning Authority prior to the date agreed in the implementation programme.

To ensure the provision and establishment of acceptable landscape in accordance with adopted Leeds Core Strategy Policy P12, Saved Leeds UDP Review (2006) policies GP5 and LD1, Leeds Natural Resources and Waste DPD, and the NPPF.

8) If within a period of five years from the date of the planting of any tree/hedge/shrub that tree/hedge/shrub, or any replacement, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season, unless otherwise agreed in writing by the Local Planning Authority.

To ensure maintenance of a healthy landscape scheme, in accordance with adopted Leeds Core Strategy Policy P12, Saved Leeds UDP Review (2006) policies GP5 and LD1, the Leeds Natural Resources and Waste DPD, and the NPPF.

9) A landscape management plan, including long term design objectives, management responsibilities and maintenance schedules shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development. The landscape management plan shall be carried out as approved.

To ensure successful aftercare of landscaping, in accordance with adopted Leeds Core Strategy policy P12, Saved Leeds UDP Review (2006) policies GP5 and LD1, the Leeds Natural Resources and Waste DPD, and the NPPF.

10) The development shall not be occupied until all areas shown on the approved plans to be used by vehicles have been fully laid out, surfaced and drained such that surface water does not discharge or transfer onto the highway. These areas shall not be used for any other purpose thereafter.

To ensure the free and safe use of the highway in accordance with adopted Leeds Core Strategy Policy T2 and Street Design Guide SPD (2009).

11) Prior to the installation of cycle/motorcycle facilities, full details of long and short stay cycle/motorcycle parking and facilities shall be submitted to and approved in writing by the Local Planning Authority. The development shall not be occupied until the approved cycle/motorcycle parking and facilities have been provided. The facilities shall thereafter be retained for the lifetime of the development.

In order to meet the aims of adopted Leeds UDP Review (2006) policies T7A, and T7B, Leeds Core Strategy Policy T1, the Travel Plans SPD and the NPPF.

12) Prior to the installation of bin stores, full details (including siting, materials and means of enclosure) of the proposed bin store(s) shall be submitted to and approved in writing by the Local Planning Authority, including provision for recycling. The bin store(s) shall thereafter be retained and maintained as such in accordance with the approved details, unless otherwise agreed in writing by the Local Planning Authority. Bins shall be stored in the approved location and not externally, except immediately before and after collection.

In the interests of amenity and visual amenity, in accordance with Leeds UDPR Policy GP5, Leeds Core Strategy Policies T2 and P10 and the NPPF.

14) Prior to the installation of car park access controls, details of the access controls to the car park shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be installed prior to first occupation of the development and retained as such thereafter, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of pedestrian and vehicular safety, in accordance with Leeds UDPR Policy GP5 and Leeds Core Strategy Policy T2

15) Prior to the first occupation of the development hereby approved, details including the locations of the proposed electric vehicle charging points shall be submitted to and approved in writing by the Local Planning Authority. The charging points shall be provided in accordance with the approved details for use prior to first occupation of the development, and retained as such thereafter.

In the interests of encouraging more sustainable forms of travel, in accordance with the NPPF, Leeds Natural Resources and Waste DPD, Leeds Travel Plans SPD, Leeds UDPR Policies GP5 and Leeds Core Strategy Policy T2

16) No development shall take place until details for the provision of relevant off-site highways works as shown on Fore Consulting drawing nos. have been submitted to and approved in writing by the Local Planning Authority for inclusion in the section 278 Highways Agreement or to be secured by such other procedure as may be agreed between the applicants and the Local Planning Authority.

In the interests of sustainable development, and vehicular and pedestrian safety, in accordance with Leeds Core Strategy Policies T1 and T2, and the NPPF.

17) Development shall not commence until a scheme detailing separate surface water and foul drainage works has been submitted to and approved in writing by the Local Planning Authority. This shall include drainage plans and summary of calculations and investigations. The works shall be implemented in accordance with the approved scheme before the development is brought into use.

To ensure sustainable drainage and flood prevention in accordance with Leeds UDP Review (2006) Policy GP5 the Council's Minimum Development Control Standards for Flood Risk, Leeds Core Strategy Policy EN5, the Leeds Natural Resources and Waste DPD and the NPPF.

18) Unless otherwise approved in writing by the Local Planning Authority, there shall be no piped discharge of surface water from the development prior to the completion of the approved surface water drainage works and no buildings shall be occupied or brought into use prior to completion of the approved foul drainage works.

To ensure that no foul or surface water discharges take place until proper provision has been made for their disposal, in accordance with the Leeds NRWDPD and the NPPF.

19) The development permitted by this planning permission shall only be carried out in accordance with the approved Flood Risk Assessment (FRA) dated 27 April 2015. The mitigation measures including

- a) Identification and provision of safe route(s) into and out of the site to an appropriate safe haven;
- b) Finished floor levels are set no lower than 26.30m above Ordnance Datum (AOD); shall be fully implemented prior to occupation and subsequently in accordance with the timing/phasing arrangements embodied within the scheme, or within any other period as may subsequently be agreed, in writing, by the Local Planning Authority.

To ensure safe access and egress from and to the site and to reduce the risk of flooding to the proposed development and future occupants, in accordance with the NPPF.

20) If remediation is unable to proceed in accordance with the approved Remediation Statement, or where significant unexpected contamination is encountered, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. An amended or new Remediation Statement shall be submitted to, and approved in writing by, the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Statement.

To ensure that any necessary remediation works are identified to protect the waterway and make the site suitable for use in accordance with policies Land 1 of the Natural Resources and Waste Local Plan 2013 and GP5 of the Unitary Development Plan Review 2006.

21) Remediation works shall be carried out in accordance with the approved Remediation Statement. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be suitable for use in accordance with policies Land 1 of the Natural Resources and Waste Local Plan 2013 and GP5 of the Unitary Development Plan Review 2006.

22) Prior to the commencement of development an updated Sustainability Statement shall be submitted to and approved in writing by the Local Planning Authority, which will include a detailed scheme comprising (i) a recycled material content plan (using

the Waste and Resources Programme's (WRAP) recycled content toolkit); (ii) a Site Waste Management Plan (SWMP); (iii) Details of photovoltaic cells; (iv) an energy plan showing the percentage of on-site energy that will be produced by the selected Low and Zero Carbon (LZC) technologies and that it produces a minimum of 10% of total demand and a carbon reduction target and plan showing a minimum of 25% betterment of the building regulations emissions target rating; and (v) a BREEAM Very Good design stage pre-assessment

The development shall be carried out in accordance with the detailed scheme, and

(a) Within 6 months of the first occupation of the development a post-construction review statement shall be submitted to and agreed in writing by the Local Planning Authority

(b) The development and buildings comprised therein shall be maintained thereafter and any repairs shall be carried out all in accordance with the approved detailed scheme and post-completion review statement or statements.

To ensure the adoption of appropriate sustainable design principles in accordance with Leeds Core Strategy Policies EN1 and EN2, Leeds SPD Sustainable Design and Construction, and the NPPF.

23) No installation of externally mounted plant or equipment shall take place until details of the installation and/or erection of any air conditioning or extract ventilation system, flue pipes, window cleaning equipment, guardrails or other excrescences proposed to be located on the roof or sides of the building, including details of their siting, design, noise attenuation, and external appearance have been submitted to and approved in writing by the Local Planning Authority. Works shall be carried out in accordance with the approved details and retained as such thereafter.

In the interests of amenity and visual amenity, in accordance with Leeds UDPR Policies GP5, BD2 and BD4, Leeds Core Strategy policy P10 and the NPPF.

24) The noise rating level from fixed plant items shall not exceed the prevailing background (LA90) noise level minus 5 dB at nearby noise sensitive receptors, when assessed in accordance with BS 4142:1997.

In the interests of residential amenity, in accordance with Leeds Core Strategy, Leeds Saved UDPR Policy GP5 and the NPPF

25) The development shall be carried out in accordance with the approved Construction Management Plan.

In the interests of highways safety and residential amenity, in accordance with Saved Leeds UDPR Policy GP5, Leeds Core Strategy Policy T2 and the NPPF.

26) No building operations shall take place before 08.00 hours on weekdays and 09.00 hours on Saturdays nor after 18.30 hours on weekdays and 13.00 on Saturdays, with no works on Sundays or Bank Holidays, unless agreed in writing with the Planning Local Authority.

In the interests of residential amenity, in accordance with Leeds Core Strategy, Leeds UDPR Saved Policy GP5 and the NPPF

27) The gradient of all pedestrian ramps shall meet BS8300:2009+A1:2010.

In the interests of accessibility in accordance with Leeds Core Strategy Policy P10

- 28) Once occupied the hours of delivery to and from the school shall be restricted to 0700 hours to 2000 hours Monday to Saturday and with no deliveries on Sundays and Bank Holidays, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of residential amenity, in accordance with Leeds UDP Review 2006 Saved Policy GP5 and the NPPF.

- 29) Prior to the first occupation of the development, a plan to identify bird nesting opportunities (for species such as House Sparrow and Starling) on the site shall be submitted to and approved in writing by the Local Planning Authority. The agreed plan shall show the number and specification of the bird nesting features, where they will be located, and a timescale for implementation. The approved details shall be implemented within the timescales agreed and retained as such thereafter.

To enhance biodiversity in the area, in accordance with the NPPF.

- 30) The development shall not be occupied until a Car Park and Servicing Management Plan has been submitted to and approved in writing by the Local Planning Authority. The plan shall be fully implemented and operated in accordance with the approved timescales.

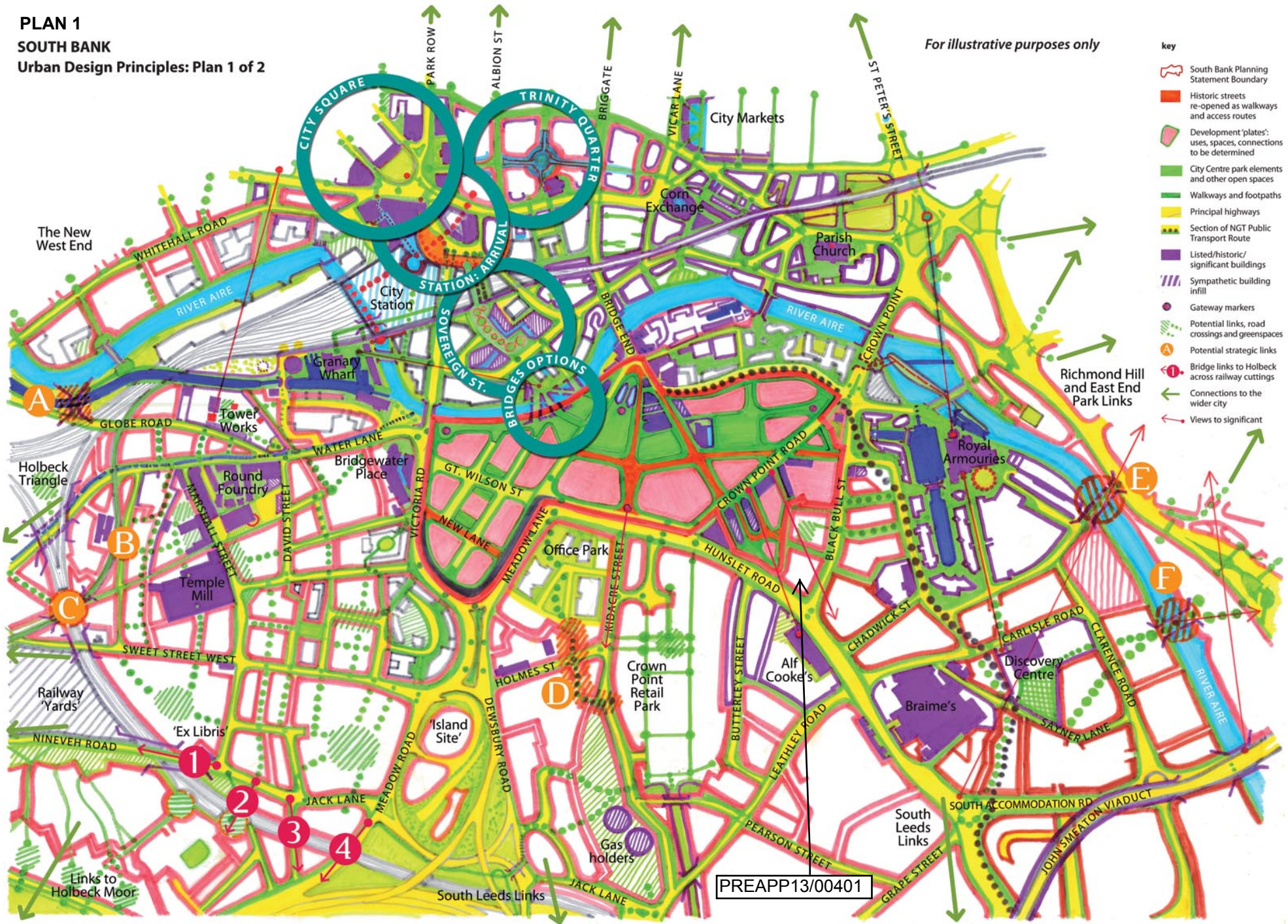
To ensure the free and safe use of the highway in accordance with adopted Leeds UDP Review (2006) policy T2

- 31) Surface water from vehicle parking and hardstanding areas shall be passed through an interceptor of adequate capacity prior to discharge to the public sewer. Roof drainage should not be passed through any interceptor.

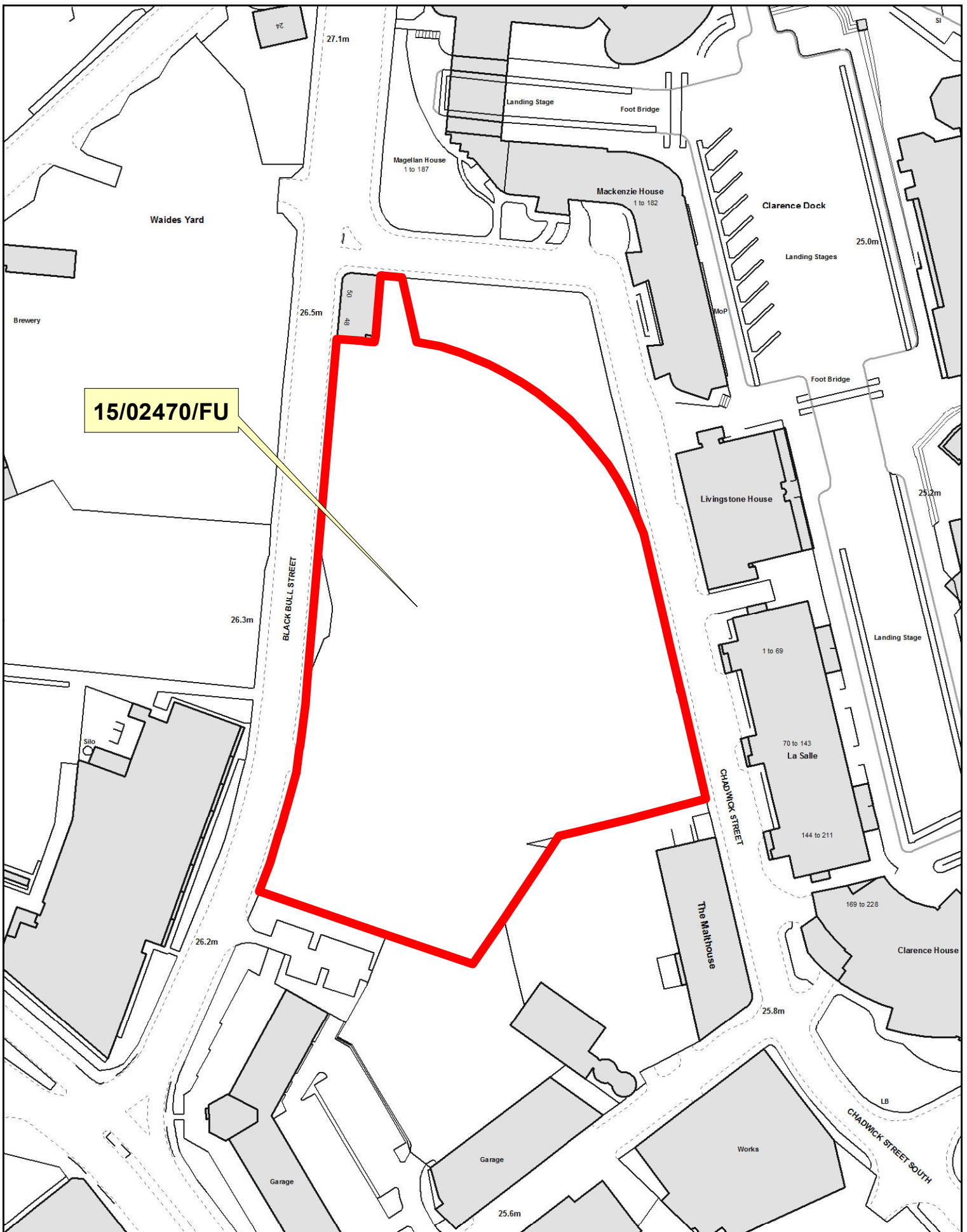
In the interest of satisfactory drainage

PLAN 1 SOUTH BANK Urban Design Principles: Plan 1 of 2

For illustrative purposes only



- key**
- South Bank Planning Statement Boundary
 - Historic streets re-opened as walkways and access routes
 - Development 'plates': uses, spaces, connections to be determined
 - City Centre park elements and other open spaces
 - Walkways and footpaths
 - Principal highways
 - Section of NGT Public Transport Route
 - Listed/historic/significant buildings
 - Sympathetic building infill
 - Gateway markers
 - Potential links, road crossings and greenspaces
 - Potential strategic links
 - Bridge links to Holbeck across railway cuttings
 - Connections to the wider city
 - Views to significant



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CITY PLANS PANEL

